

STATE OF NEBRASKA
DEPARTMENT OF ROADS
ADDENDUM NO. 1
AND
ELECTRONIC BIDDING SYSTEM
AMENDMENT NO. 1
PROJECT NOS. IM-NH-80-9(830), IM-NH-80-9(835)
IM-NH-80-9(836) & IM-NH-80-9(842)
CONTROL NOS. 12457, 12462, 12463 & 12469
CALL NO. 110
I-80, GREENWOOD CREEK BRIDGES, DEE CREEK BRIDGES,
CAMP CREEK BRIDGES & GREENWOOD - WAVERLY
LETTING DATE: OCTOBER 8, 2009

The Schedule of Items for Project IM-NH-80-9(830) is amended as follows:

- 1 - 4. In Groups 6, 6A, 6B and 6C, the bid item "Access Bridge" (Item No. 6052.52) has been added with a quantity of 1.000 LS.

The Schedule of Items for Project IM-NH-80-9(835) is amended as follows:

- 5 – 8. In Groups 6D, 6E, 6F and 6G, the bid item "Access Bridge" (Item No. 6052.52) has been added with a quantity of 1.000 LS.

The Schedule of Items for Project IM-NH-80-9(836) is amended as follows:

- 9 – 12. In Groups 6H, 6I, 6J and 6K, the bid item "Access Bridge" (Item No. 6052.52) has been added with a quantity of 1.000 LS.

The Schedule of Items for Project IM-NH-80-9(842) is amended as follows:

13. In Group 1, the quantity for the bid item "Earthwork Measured in Embankment" (Item No. 1030.00) is incorrect and should read 740,493.000 CY.
14. In Group 4, the quantity for the bid item "Excavation of Pipe, Pipe-Arch Culverts, and Headwalls" (Item No. 4050.01) is incorrect and should read 14,580.00 CY.
15. In Group 4, the quantity for the bid item "24" Metal Flared-End Section" (Item No. 4360.24) is incorrect and should read 2.000 EACH.
16. In Group 4, the quantity for the bid item "48" Culvert Pipe, Type 3, 4 or 5" (Item No. P300.48) is incorrect and should read 493.000 LF.
17. In Group 4, the quantity for the bid item "30" Culvert Pipe, Type 3, 4, 5 or 6" (Item No. P402.30) is incorrect and should read 128.000 LF.
18. In Group 4, the bid item "24" Culvert pipe, Type 3, 4, 5 or 6" (Item No. P402.24) has been added with a quantity of 108.000 LF.

19. In Group 4, the quantity for the bid item "48" Metal Flared-End Section" (Item No. 4360.48) is incorrect and should read 16.000 EACH.

The EBS generated bid items sheets must show these corrections or the bid will be considered void.

* * * * *

On page 64 of the Special Provisions, the provision titled **SPECIAL PROSECUTION AND PROGRESS (Contractor Access Pavement)** is amended as follows:

1. The first sentence of paragraph one is amended to read:

The plans include Contractor Access and Deceleration pavement for the purpose of allowing access to the construction area during Phase 3.

2. The last paragraph is amended to include the following:

See **Section III.C.** in the provision titled **SPECIAL PROSECUTION AND PROGRESS (Phasing)**.

* * * * *

On pages 65 – 67 in the Special Provisions, the provision titled ENVIRONMENTAL COMMITMENT is amended to include the following as depicted on the attached sheets:

1. NDOR Request to USACE for access bridges.
2. USACE Approval for access bridges.

* * * * *

On page 107 of the Special Provisions, in the provision titled **CRUSH CONCRETE PAVEMENT**, the two paragraphs following the table are void and superseded by the following:

Concrete crushing sites are available and located at either the Greenwood Maintenance Yard or the Waverly Interchange, as directed by the Engineer. The Contractor may use one or both of these sites for the crushing operation. However, the Waverly Interchange shall be used to stockpile all excess crushed concrete and milled asphalt from the project; and at the completion of the project, any remaining stockpiled material at the Greenwood Maintenance Yard shall be moved to the Waverly Interchange stockpile site at no cost to the State. The crushing site(s) and the stockpile site(s) shall not be used as a construction waste dumping area.

The Contractor is expected to vacate the crushing site in a timely manner at the completion of the crushing operation and the site is to be left in a clean, neat and orderly manner.

* * * * *

On pages 107 and 108 of the Special Provisions, in the provision titled **FOUNDATION COURSE 6**", the third paragraph is void and superseded by the following:

The State has made available at no cost to the Contractor millings and salvaged crushed concrete for use as Foundation Course on the project. This material may be obtained from existing stockpile sites located at the Greenwood Maintenance Yard and the Waverly Interchange, or from the Crushed Concrete and the Cold Milling operations which are part of the project, as directed by the Engineer. Any processing of stockpiled material necessary to meet the requirements for Crushed Concrete Foundation Course or Bituminous Foundation Course will not be measured and paid for but shall be considered subsidiary to the item "Foundation Course 6".

In Phase 2, the Contractor is directed to use stockpiled millings for the construction of the eastbound pavement. Approximately 63,000 tons of millings are available from the Waverly Interchange and approximately 7,800 tons of millings are available from the Greenwood Maintenance Yard.

In Phase 3, the Contractor is directed to use any remaining stockpiled millings from the Waverly Interchange and the Greenwood Maintenance Yard in addition to millings produced from the project to complete the eastbound pavement.

In Phase 4, any unused stockpiled millings/crushed concrete, remaining millings from the project and crushed concrete from the project shall be used to construct the westbound lanes.

* * * * *

The Special Provisions are amended to include the following:

**SPECIAL PROSECUTION AND PROGRESS
(Hauling Millings From The Waverly Interchange)**

The existing stockpile of asphaltic concrete millings is located inside the loop of the westbound on-ramp in the Waverly Interchange. In order for the Contractor to haul these millings to the eastbound lanes in Phases 2 and 3, the following shall apply:

1. The Contractor will be allowed to haul millings across the westbound on-ramp and merge with the traffic on the westbound off-ramp in order to gain access to US-6.
2. Flagging operations at both the westbound on-ramp and the westbound off-ramp shall be used at all times when millings are hauled across these ramps. At no time during these flagging operations shall traffic be allowed to back up more than 150± feet from the crossing point on the westbound on- ramp or more than 300± feet from the crossing point on the westbound off-ramp.
3. The location of the haul route location for these ramps shall be located 300 feet back towards I-80 from the outside edge of pavement of the US-6 northbound lanes.
4. The Contractor may only haul millings over these ramps during non-peak hours and between the hours of 9 am and 3 pm, unless otherwise approved by the Engineer. There will be no millings hauled over these ramps on holidays or any day of a UNL home football game.

5. Haul ramps to the westbound off-ramp and between the two westbound ramps shall be surfaced with crushed rock as a minimum. Any debris (mud, crushed rock, etc.) that is dragged onto the two ramps shall be cleaned as directed by the Engineer. The work of constructing, surfacing, maintaining the haul ramps and cleaning the two westbound ramps shall be considered subsidiary to other items of work for which direct payment is made.

* * * * *

The Special Provisions are amended to include the following:

CONTRACTOR'S ACCESS BRIDGE

It will be the Contractor's option to use an access bridge for a temporary crossing to construct the bridge on this project.

It will be the Contractor's responsibility to submit a plan of the access bridge to the Corps of Engineers for approval. Construction of the access bridge will not begin until written approval has been received from the Corps of Engineers. The Contractor should not expect to receive approval from the Corps of Engineers prior to the letting.

Bidders must submit a bid for the item "Access Bridge" in the schedule of items.

The item "Access Bridge" will be paid for as a lump sum. The bid price shall be considered full compensation for all work required for the contractor to construct and remove the access bridge. The Contractor will only be paid for this item if they construct the access bridge. The Contractor will be paid 90% of the lump sum when the access bridge is installed. The remaining 10% of the lump sum will be paid when the access bridge is removed.

If the Contractor does not plan to utilize an access bridge, they shall bid the item "Access Bridge" at \$0. If the contractor bids this item at \$0 and later decides to utilize an access bridge, it will be at the contractor's expense.

No change orders will be approved to increase the cost of the "Access Bridge" item after award of the contract.

* * * * *

The Special Provisions are amended to include the following:

EXISTING CONCRETE PAVEMENT

The existing concrete pavement on this project contains mesh reinforcement.

* * * * *

On plan sheets 3 and 33 of Project IM-NH-80-9(830), the QUANTITIES columns for Groups 6, 6A, 6B and 6C are amended to include the following:

Access Bridge	1 Lump Sum
---------------	------------

* * * * *

On plan sheets 3 and 36 of Project IM-NH-80-9(835), the QUANTITIES columns for Groups 6D, 6E, 6F and 6G are amended to include the following:

Access Bridge 1 Lump Sum

* * * * *

On plan sheets 3 and 35 of Project IM-NH-80-9(836), the QUANTITIES columns for Groups 6H, 6I, 6J and 6K are amended to include the following:

Access Bridge 1 Lump Sum

* * * * *

On plan sheet 2-N2 of Project IM-NH-80-9(842), the EARTHWORK QUANTITIES table is void and superseded by the table depicted on the attached sheets.

* * * * *

On plan sheet 2-N4 of Project IM-NH-80-9(842), Legend item (4) is amended to read:

(4) Filter Fabric (Filter Fabric ends at the interception of 6:1 slope and the granular subdrain)

* * * * *

On plan sheets 2-P47 and 2-P48 of Project IM-NH-80-9(842), the strip of surfacing shown in the median area of the eastbound lanes as "Completed Crushed Rock Embedment", from Sta. 1434+10± to Sta. 1451+50±. is incorrect and should be shown as "Completed Concrete Pavement/Bridge".

It should be noted that the quantities associated with this work are correct.

* * * * *

On plan sheets 2-L14 and 151 of Project IM-NH-80-9(842), the size of the Area Inlet at Sta. 1419+99.80-137.2' Lt will be redesigned and the quantities adjusted as part of the plan revision resulting from this addendum.

* * * * *

On plan sheet 81 of Project IM-NH-80-9(842), the following changes are made in the OVERHEAD CANTILEVER STRUCTURES table:

1. For the structure at Location No. 36, the station is incorrect and should read 1260+00.
2. For the structure at Location No. 36, the H dimension is incorrect and should read 1150.23.

* * * * *

Plan sheet 84 of Project IM-NH-80-9(842) is void and superseded by added sheet 84A as depicted on the attached sheet.

Due to the reproduction size of added sheet 84A, it should be pointed out that the following items are the only additions to sheet 84A:

1. Note 12 is added and reads as follows:

Contractor shall install noted power conduit and reconnect power to the existing DMS System as the first order of work in 2009. Contractor shall be responsible for any damage to the current or new DMS System as a result of project work. Noted conduit work and additional work required to reconnect power to existing DMS System shall be subsidiary to the pay item Install Dynamic Message Sign.

2. The items to be installed in 2009 are indicated by the asterisked (*) notes on added sheet 84A.
3. DMS dimensions have been added.
4. Pavement dimensions (**) have been added to the OVERHEAD SIGN SUPPORT detail.

* * * * *

It should be noted that two additional sheets (labeled as "ADDITIONAL DETENTION PONDS @ 176th STREET BRIDGE" and "ADDITIONAL CULVERTS") are included with the attached sheets. These "for information only" sheets depict where the additional grading and culverts for these detention ponds are located. The plans will be revised to include this information as part of the plan revision resulting from this addendum.

* * * * *

Upon execution of the contract, the plans will be revised to reflect these changes.

DEPARTMENT OF ROADS

Original Signed by Liz Wunderlich

for Claude Oie
Construction Engineer

Issued: September 30, 2009

CO: 110AD110

NOTICE: Only the contractors issued bidding proposals receive this addendum and responsibility for notifying any potential subcontractors or suppliers remains with the contractor.



Dave Heineman
Governor

STATE OF NEBRASKA

DEPARTMENT OF ROADS
John L. Craig, Director

1500 Highway 2 • PO Box 94759 • Lincoln NE 68509-4759
Phone (402)471-4567 • FAX (402)479-4325 • www.dor.state.ne.us

August 27, 2009

Keith Tillotson
US Army Corps of Engineers
2214 2nd Avenue
Kearney, NE 68847-5315

RE: USACE # 2008-03077 Amendment Request
Waverly Interchange to Greenwood Interchange
80-9(842) / CN 12469

The Nebraska Department of Roads (NDOR) is constructing a roadway improvement project on Interstate 80 (I-80) in Lancaster and Cass Counties. The project will involve the improvement of a six lane system from the current four lane system, and adjoining I-80 bridges over several creeks.

NDOR is requesting an amendment to the authorized permit (USACE# 2008-03077) to install temporary bridges at three locations: Site 11--Camp Creek, Site 15--Dee Creek, and Site 20--Greenwood Creek. The temporary crossings will consist of crane mats and support beams that will span the creek channels above their Ordinary High Water Marks (OHWM). When the span is in excess of 30 feet, temporary pile may be necessary to support the structure and will occur below the OHWM. The maximum width of the crossing would be no more than 30 feet (Reference Attachments).

Access roads will be used in conjunction with the crossings, which will be above the OHWM. Due to the depth of the incised channel the channel side slopes will be excavated down to give the contractor less of an incline when accessing the temporary bridge crossings. Following construction, the temporary bridges will be removed and the side slopes restored bank to the existing grade. Impacted wetlands that were previously reported to be temporarily impacted will be permanently impacted as a result of the temporary bridges and access roads as follows:

Site 11—Camp Creek
0.021 acres of Riverine Channel PEMA

Site 15—Dee Creek
0.012 acres of Riverine Channel PEMA

Site 20—Greenwood Creek
0.036 acres of Riverine Channel PEMA

U.S. Army Corps of Engineers
August 27, 2009
Page 2

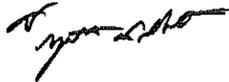
The original project had a total of 2.867 acres of PEMA/C and PFOA1 permanent wetland impacts, for which NDOR debited 3.372 acres of Riverine Floodplain PEMA/C wetlands from the Lincoln-Davey Wetland Mitigation Bank site ledger and 1.21 acres of Floodplain Depressional PEMA/C wetlands from the Rock Creek Wetland Mitigation Bank site ledger totaling 4.582 acres.

If mitigation is required for the additional impacts, NDOR proposes to mitigate the 0.069 acres of Riverine Channel PEMA wetlands at a 2:1 ratio at the Rock Creek Wetland Mitigation Bank site totaling 0.138 acres (Reference Attached Mitigation Ledger).

If you have any questions concerning the project described above, please call me at 402-479-3567.

Respectfully Submitted,

NEBRASKA DEPARTMENT OF ROADS

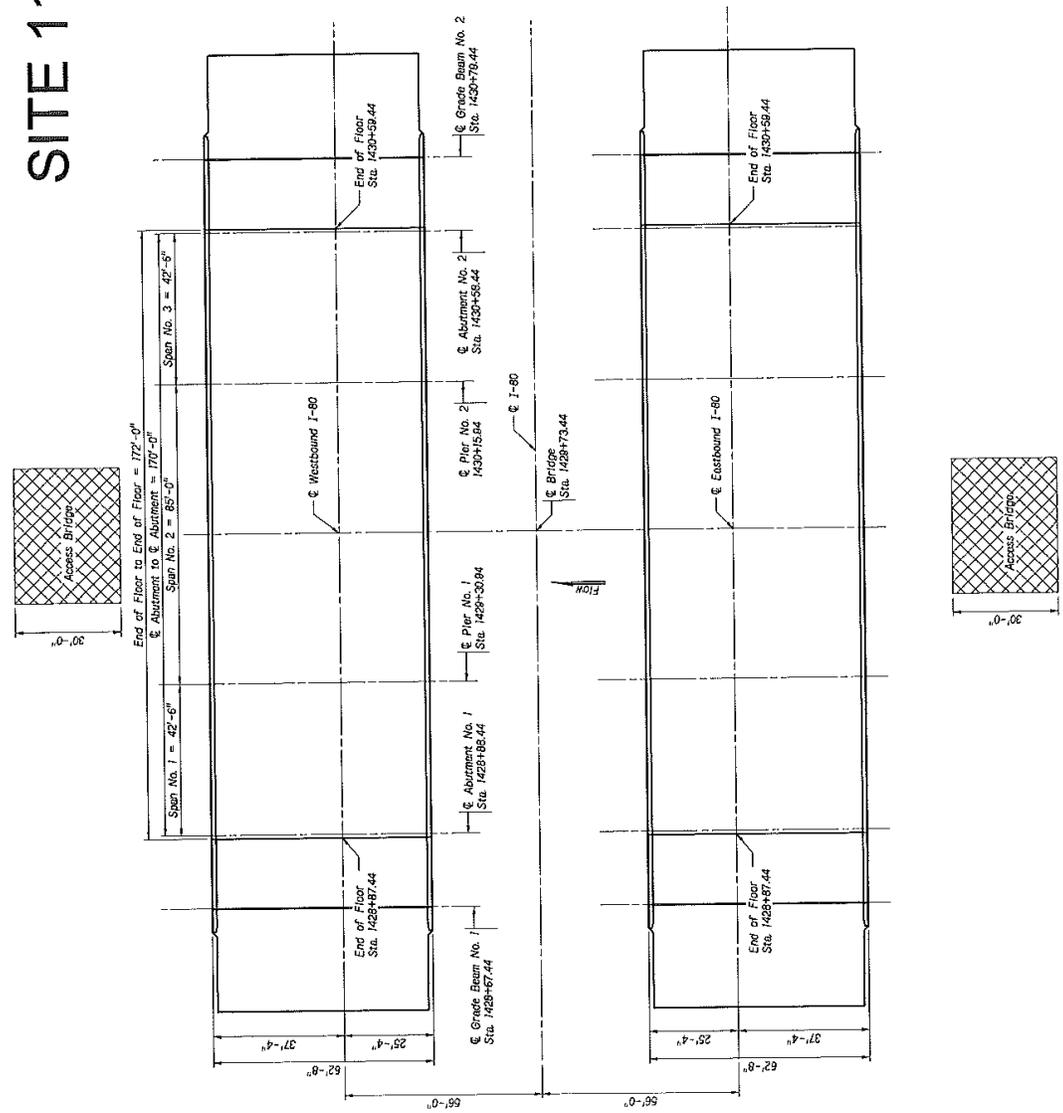


Wyatt R. Webster
Highway Environmental Biologist

WRW/wrw
12469 – Amendment Request

PROJECT NUMBER 80-9(835)	SHEET NO. 1	TOTAL SHEETS 32	PROJECT NUMBER 80-9(835)	DRAWING NUMBER 12453	PRODUCTION NUMBER 3080 414654 3080 414654	STATE OF NEBRASKA - DEPARTMENT OF ROADS - BRIDGE DIVISION DATE: APRIL 2009 CHECKED BY: JJS/CPB DRAWN BY: JJS	COUNTY LANCASTER LOCATION: CAMP CREEK BRIDGES SECT 0° RFP POST: 41465 STA: 1429+73.44 DESIGN LR LOAD HL93 ROADWAY 2 - 60'-0" ACCESS BRIDGE	COUNTY LANCASTER LOCATION: CAMP CREEK BRIDGES SECT 0° RFP POST: 41465 STA: 1429+73.44 DESIGN LR LOAD HL93 ROADWAY 2 - 60'-0" ACCESS BRIDGE
-----------------------------	----------------	--------------------	-----------------------------	-------------------------	---	---	---	---

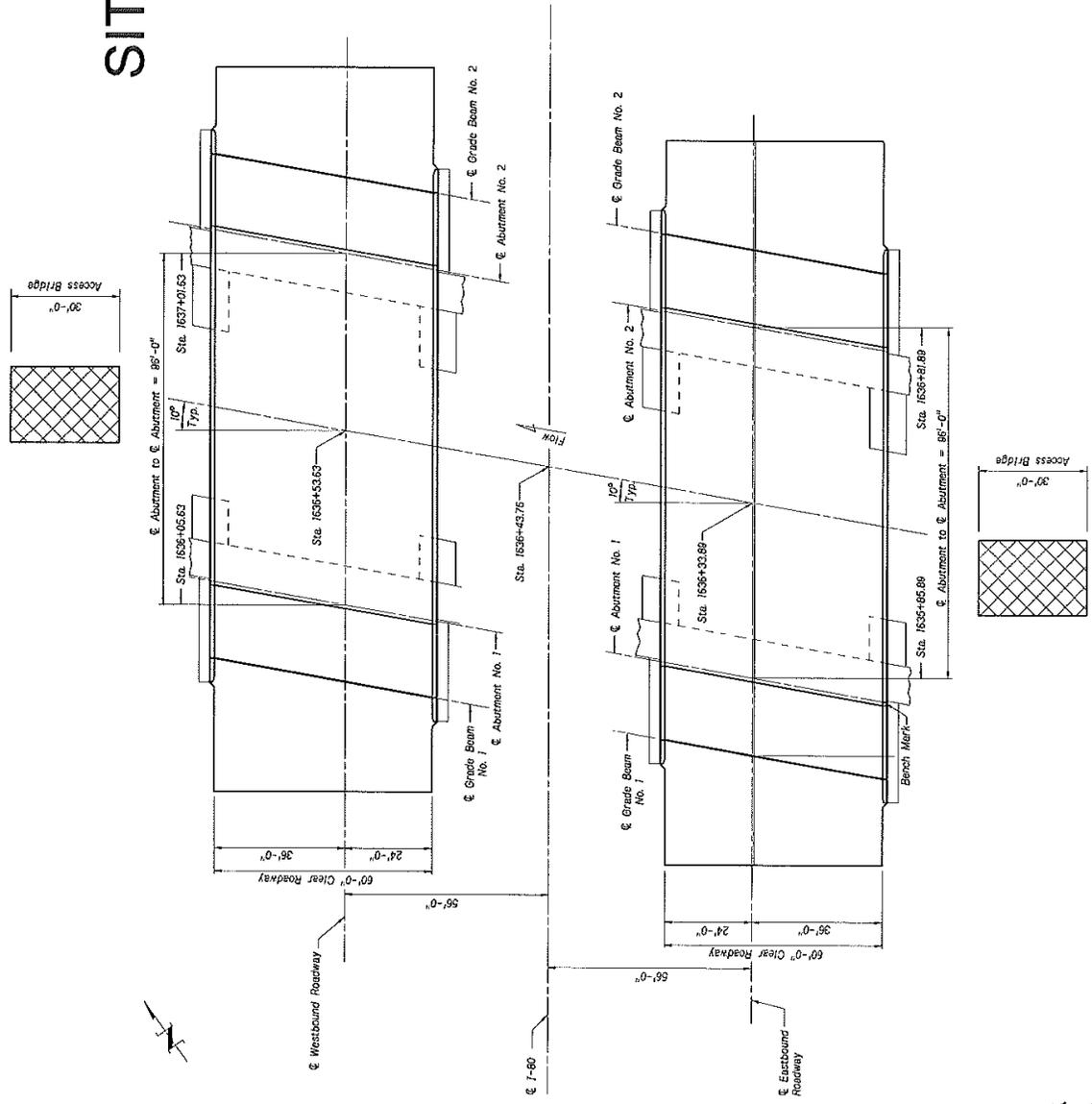
SITE 11



ACCESS BRIDGE
 Scale: 1" = 16'-0"

PROJECT NUMBER 80-9 (830) 34	SHEET 34
C.N. 12457	
STRUCTURE NUMBER S080 41896L&R	
STATE OF NEBRASKA DEPARTMENT OF ROADS - BRIDGE DIVISION	
DATE	CHECKED BY
DESIGNED BY	DRAWN BY
STA. 1636+43.76	STA. 1636+43.76
DESIGN LIVE LOAD	DESIGN LIVE LOAD
ROADWAY 2-60'-0"	ROADWAY 2-60'-0"
SKEW 10° RHD	SKEW 10° RHD
BRIDGES	BRIDGES
LOCATION GREENWOOD CREEK	LOCATION GREENWOOD CREEK
COUNTY CASS	COUNTY CASS
HWT. NO. 1-80	HWT. NO. 1-80
REP. POST. 418.56	REP. POST. 418.56
96'-0" SIMPLE SPAN BRIDGES	96'-0" SIMPLE SPAN BRIDGES
ACCESS BRIDGE LOCATIONS	ACCESS BRIDGE LOCATIONS
BRIDGE ENGINEER	
REGISTERED PROFESSIONAL ENGINEER MARK L. HALL STATE OF NEBRASKA No. 0000000000	
REGISTERED PROFESSIONAL ENGINEER ANDOR No. 0000000000	
SPECIAL PLAN NO. 2	2
	30

SITE 20





REPLY TO
ATTENTION OF

DEPARTMENT OF THE ARMY
CORPS OF ENGINEERS, OMAHA DISTRICT
NEBRASKA REGULATORY OFFICE – KEARNEY
2214 2ND AVENUE
KEARNEY, NEBRASKA 68847-5315

<https://www.nwo.usace.army.mil/html/od-rne/nehome.html>

September 9, 2009

Mr. Richard Ruby
Nebraska Department of Roads
P.O. Box 94759
Lincoln, Nebraska 68509-4759

Dear Mr. Ruby:

2008-03077-KEA
Amendment #1

In accordance with your August 27, 2009 request, the authorization granted by the U.S. Department of the Army permit 2008-03077-KEA is hereby amended to include the following:

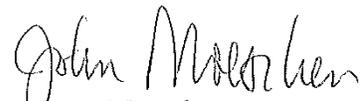
Construction access to each of three sites will result in the excavation of 0.069 acre of riverine channel wetlands with a Cowardin classification of palustrine, emergent, temporarily flooded (PEMA). The sites will be restored to pre-project elevations and contours following completion of the bridges. No additional filling activity in waters of the U.S. will occur as a result of this amendment. These secondary impacts will be mitigated through the debiting of 0.138 credit from the Rock Creek mitigation bank.

All special and general conditions in the original permit will remain valid as written. The time limit for completing the work by June 30, 2012 remains unchanged. This authorization does not obviate the need to obtain other Federal, State, Tribal or Local authorizations that may be required by law.

BY THE AUTHORITY OF THE SECRETARY OF THE ARMY:

David C. Press
Colonel, Corps of Engineers
District Commander

BY:

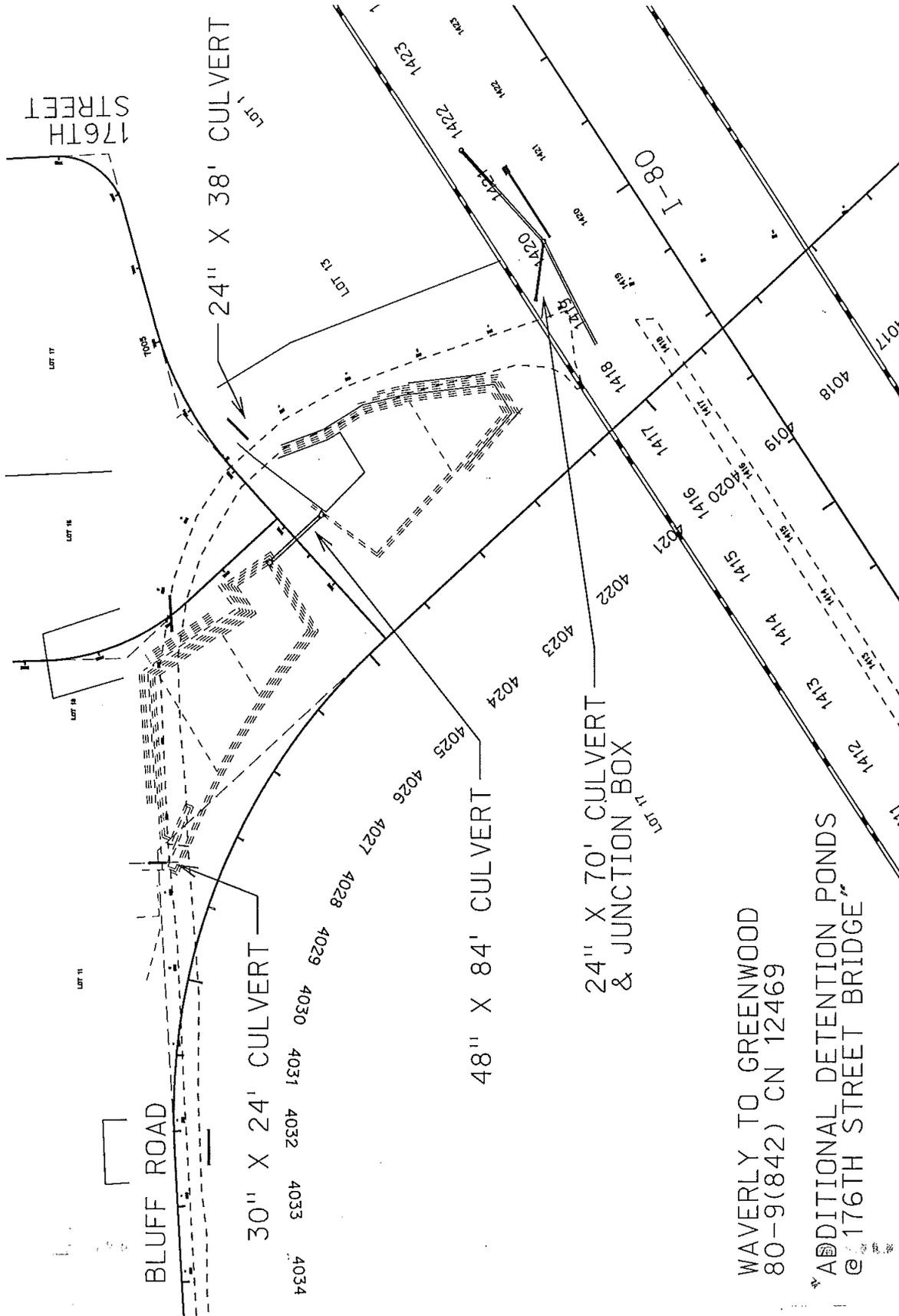

John L. Moeschel
Nebraska State Program Manager

Copy Furnished:

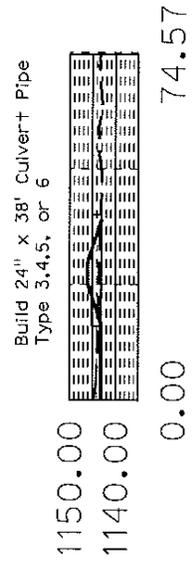
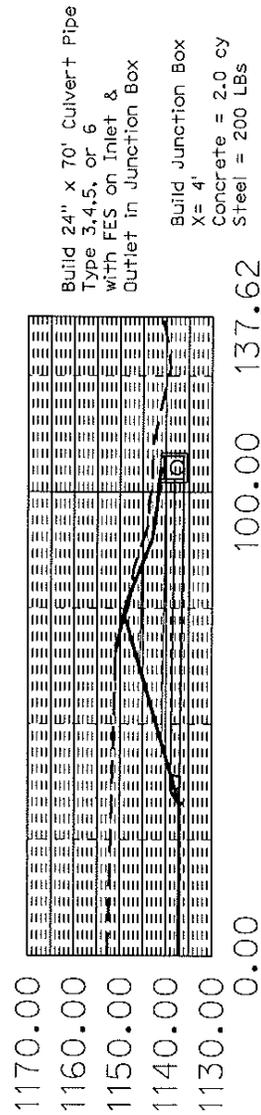
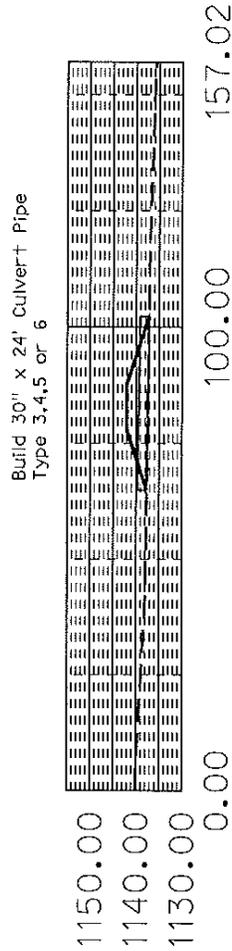
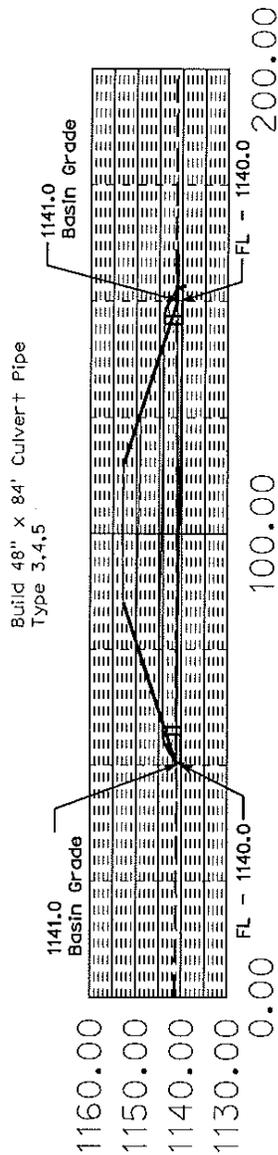
NDOR (Webster)
NDOR (Jurgens)

EARTHWORK QUANTITIES			
PHASE	ALIGNMENT	EXCAVATION AVAILABLE (CY)	EARTHWORK MEASURED IN EMBANKMENT (CY)
1	I-80	311	3834
2	I-80	315877	457156
2	WAVERLY DETENTION POND	60684	4556
2	176TH ST. DETENTION PONDS	19223	928
3	I-80	203099	7113
4	I-80	209245	219869
4	WAVERLY RAMP	2237	47037
TOTAL		810676	740493

PLAN SHEET 2-N2



MAVERLY TO GREENWOOD
80-9(82) CN 12469
ADDITIONAL DETENTION PONDS
@ 176TH STREET BRIDGE



WAVERLY TO GREENWOOD'
 80-9(842) CN 12469
 1# ADDITIONAL CULVERTS"